
Traffic Management Plan

Wetherill Park Resource Recovery Facility

Document #. PLAN002

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Version 2

Plan to specify the designated areas for various traffic flows throughout the facility.



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1. TYPES OF FLOW

1.1 Light Vehicle (Domestic) Movements

Light vehicle traffic (Domestic) includes, but is not limited to:

- General public vehicles
- General public vehicles with trailers
- Trade utilities (with or without trailers)
- Vans (with or without trailers)
- Staff vehicles

1.2 Commercial Vehicle Movements

Commercial vehicles include, but are not limited to:

- Local Council waste collection vehicles
- Third party waste collection vehicles
- Trade vehicles in excess of 3 Tonnes
- Heavy rigid vehicles up to 10 Tonnes

1.3 Waste Transport Vehicle Movements

Waste transfer vehicles include:

- Walking floor trailers used for transport of wastes to other facilities
- Hook lift vehicles used for transport of hook lift bins
- Roll On Roll Off (RORO) vehicles used for transport of RORO bins

1.4 Plant Movements

Plant movements include:

- Dozer
- Excavator (3)
- Loaders (2)
- Skidsteer
- Forklift
- Waste Transfer vehicles (4)

1.5 Pedestrian Movements

Site personnel:

- Weigh bridge staff
- Machinery/Plant operators
- Approved contractors (e.g. Mechanics, Electricians, plumbers)
- Transport operators
- Supervisory and Management staff

Customers and visitors.

2. TRAFFIC ZONES

2.1 Site Entry and Exit

- All Domestic and Commercial traffic entering and exiting the premises are directed through the site weighbridge. The weighbridge staff direct all vehicles entering the site via the weighbridge to the appropriate unloading area according to the waste type that the vehicle is carrying.
- Waste Transfer Vehicles enter the site and immediately veer left onto the Load-Out Tunnel access road.
- All visitors and contractors are required to sign in and out at Reception or the Weighbridge upon entry and exit.
- All Domestic, Commercial and Waste Transfer Vehicles exit the site via the “Out” weighbridge and then proceed onto Davis Road.

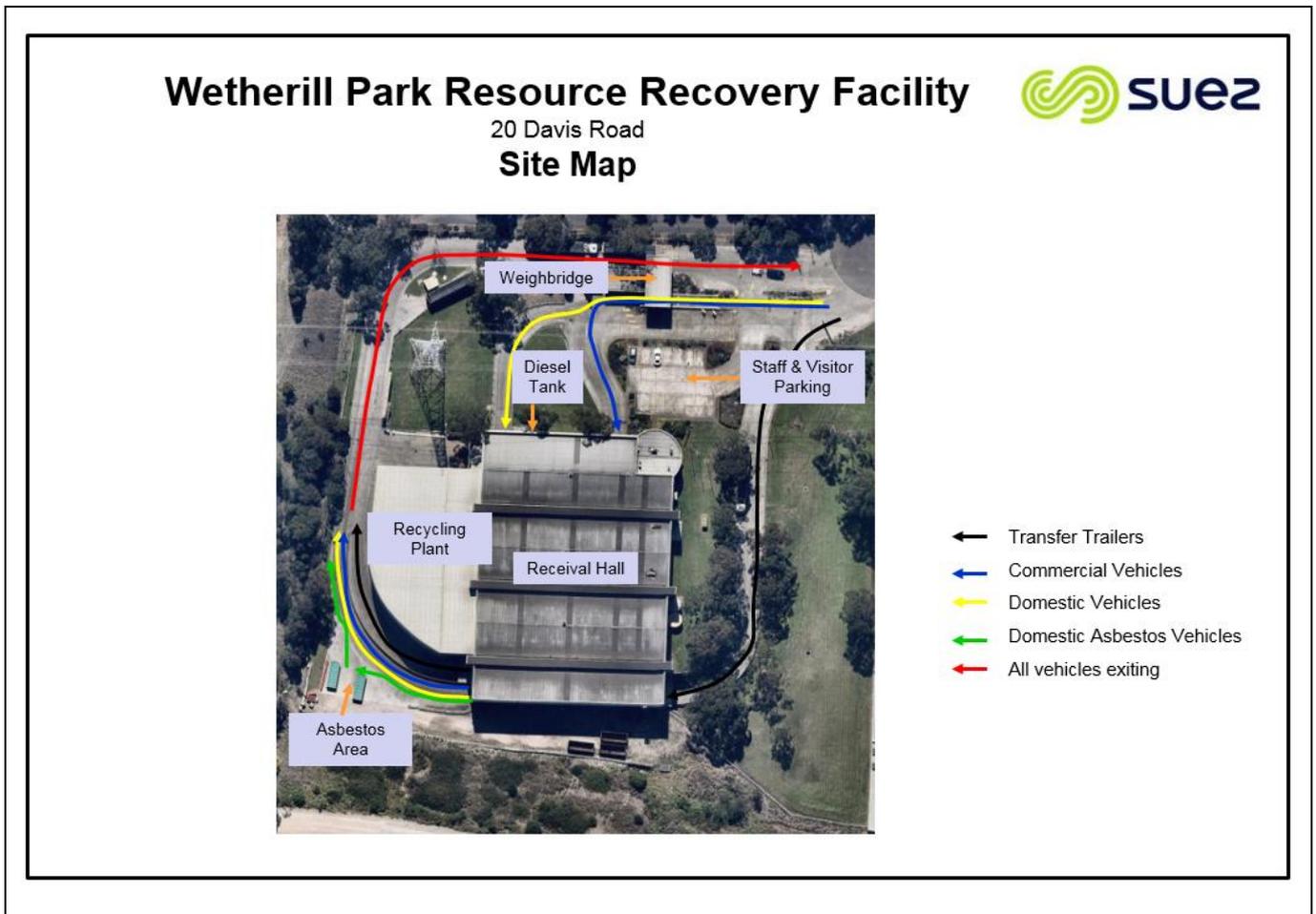


Diagram 1 – Traffic Flow

2.2 Transfer Station

- Domestic customer is directed to enter the receival hall via the western door way (Gate 1) and to tip at the appropriate area (see Diagram 2). Customers are advised of the appropriate area by the Weighbridge Operator, Operators inside the receival hall will also advise customers of where to tip, there is a map at the “In” weighbridge showing customers where to tip
- After unloading customers exit through the south western doorway and proceed to the “Out” weighbridge
- All domestic customers are told to keep 2 metres back from the edge of the pit and to tip their waste directly onto the floor beside the surge pit
- If a Domestic customer has a full load of clean cardboard they are instructed to enter the Receival Hall via the eastern doorway (Gate 2) and to proceed to the south eastern corner to tip off their cardboard, the weighbridge operator will communicate this to the operators within the receival hall via the 2-way
- Domestic Asbestos customers are informed by the weighbridge operator to enter the receival hall via the western doorway (Gate 1) and proceed through the receival hall exiting the south western doorway and to proceed to the left to the asbestos area where they will be met by an Operator who will open the asbestos bin and supervise the unloading of asbestos and reject any loads that are not wrapped correctly or to large. The customer is not to leave the weighbridge until the weighbridge operator has confirmation from an operator within the receival hall that there is someone available to supervise the unload.
- All Commercial loads are directed to enter via the eastern doorway (gate 2) and to tip off on the floor on the western side of the surge pit, loads of putrescible waste are to be tipped off at the front (southern end) of the surge pit (this waste is not to be tipped directly into the surge pit).

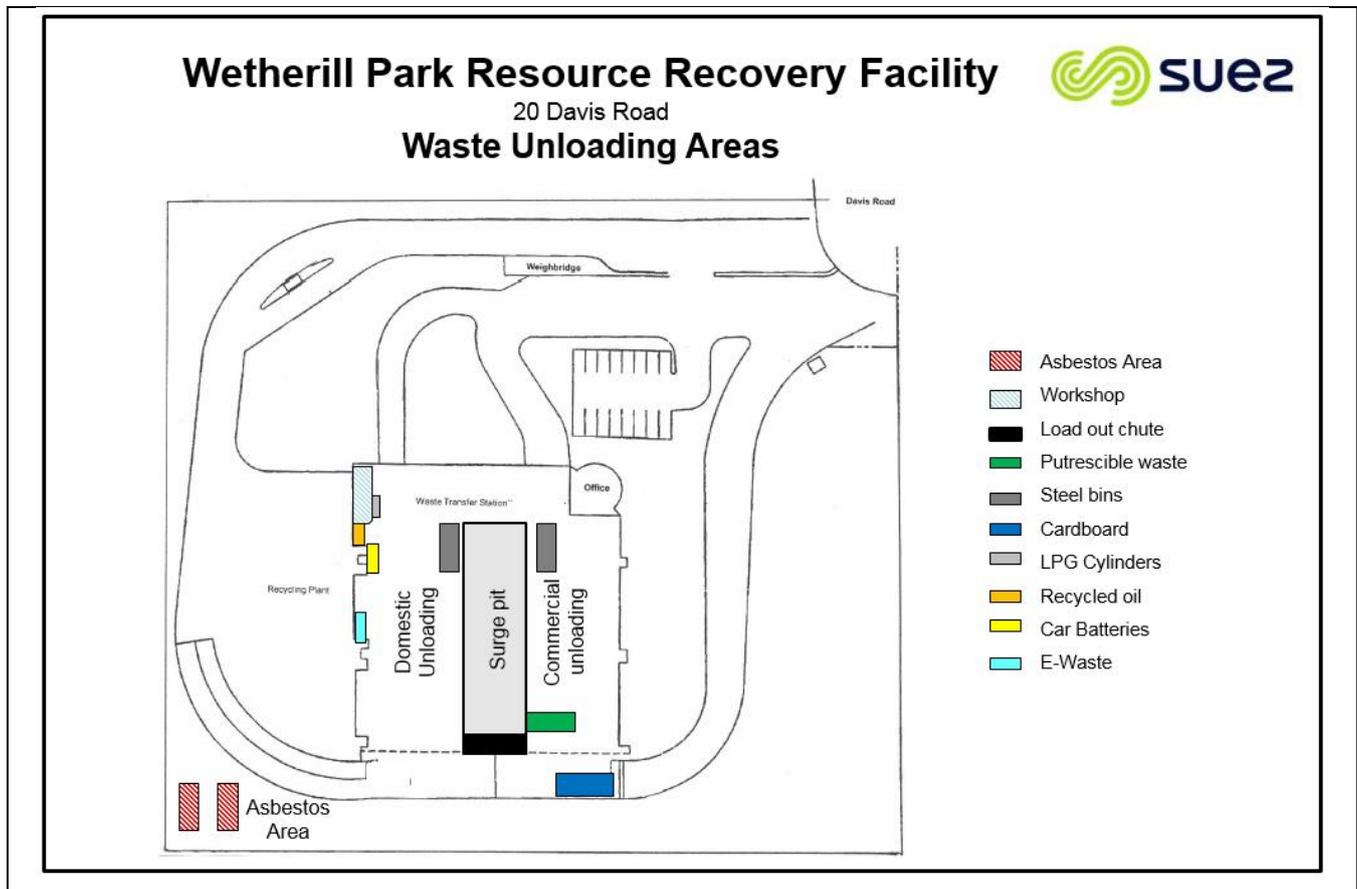
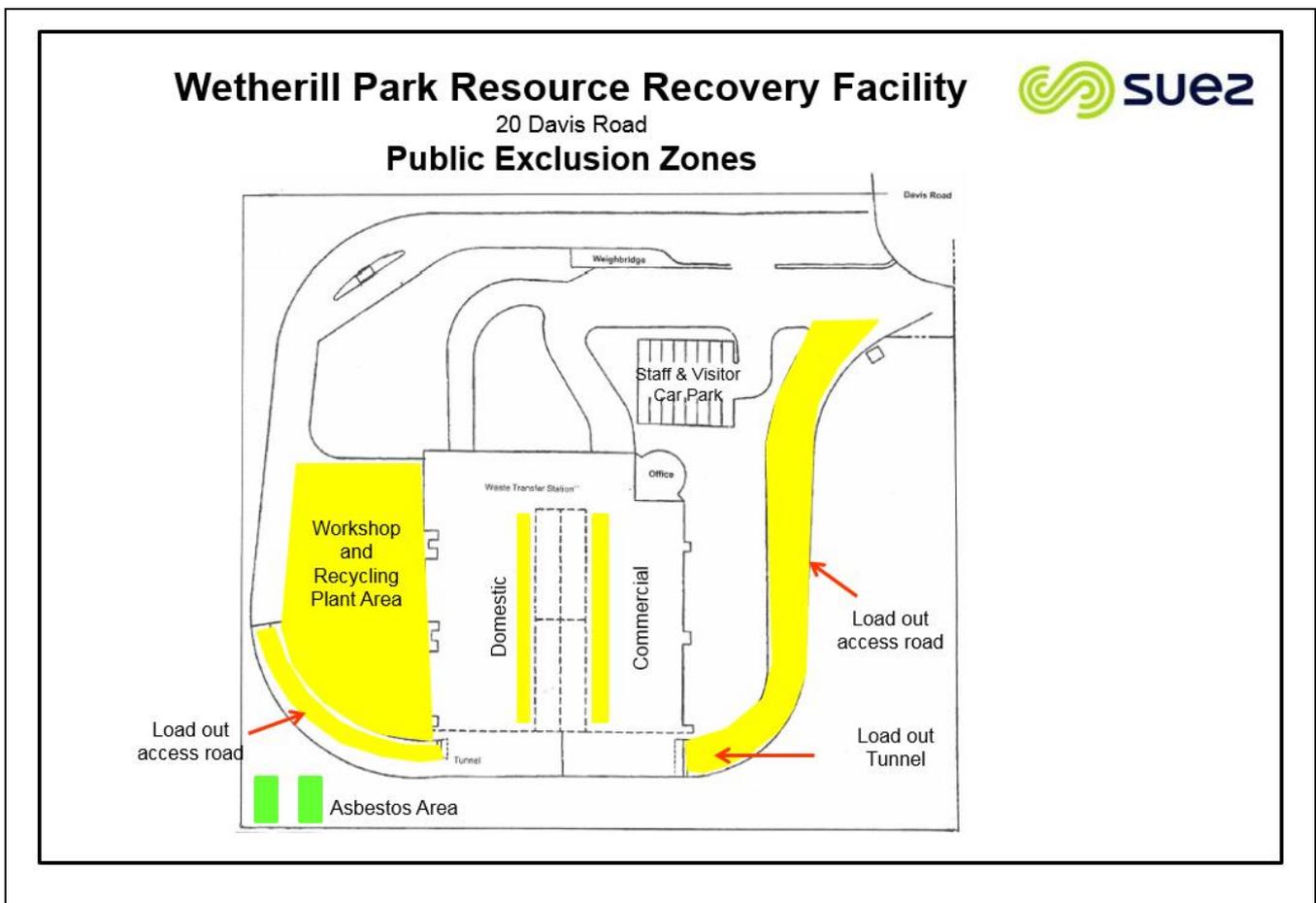


Diagram 2 – Waste unloading areas

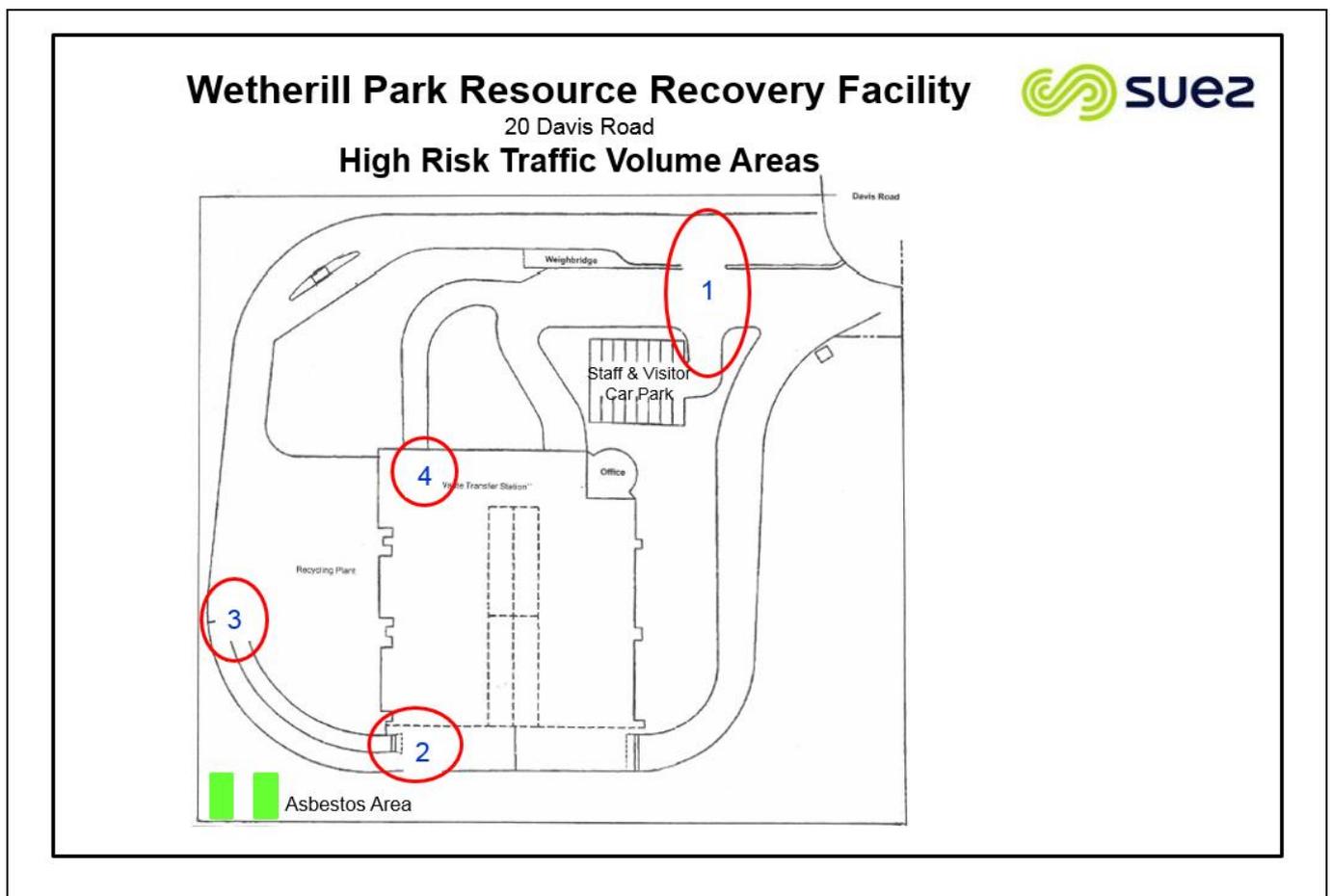
2.5 Public Exclusion Zones

- Load out access road
- Load out tunnel
- Exiting load out access road
- Workshop
- Recycling Plant
- Domestic customers are not permitted on the edge of the surge pit



2.6 High Risk Traffic Volume Areas

- Traffic exiting from staff and visitor carpark, crossing traffic flow of vehicles entering and exiting the site
- Domestic and Commercial traffic exiting reception hall
- Domestic, Commercial, Asbestos Customers and Waste Transfer Vehicles merging on the exit road.
- Domestic and Fuel Tanker during refuelling



2.7 Shared Traffic Areas

- Reival Hall Commercial side – Commercial Vehicles and Plant
- Reival Hall Domestic side – Domestic Vehicles and Plant
- Exit Road – Commercial and Domestic Vehicles with Waste Transfer Vehicles
- Bobcat is used on all roads for cleaning

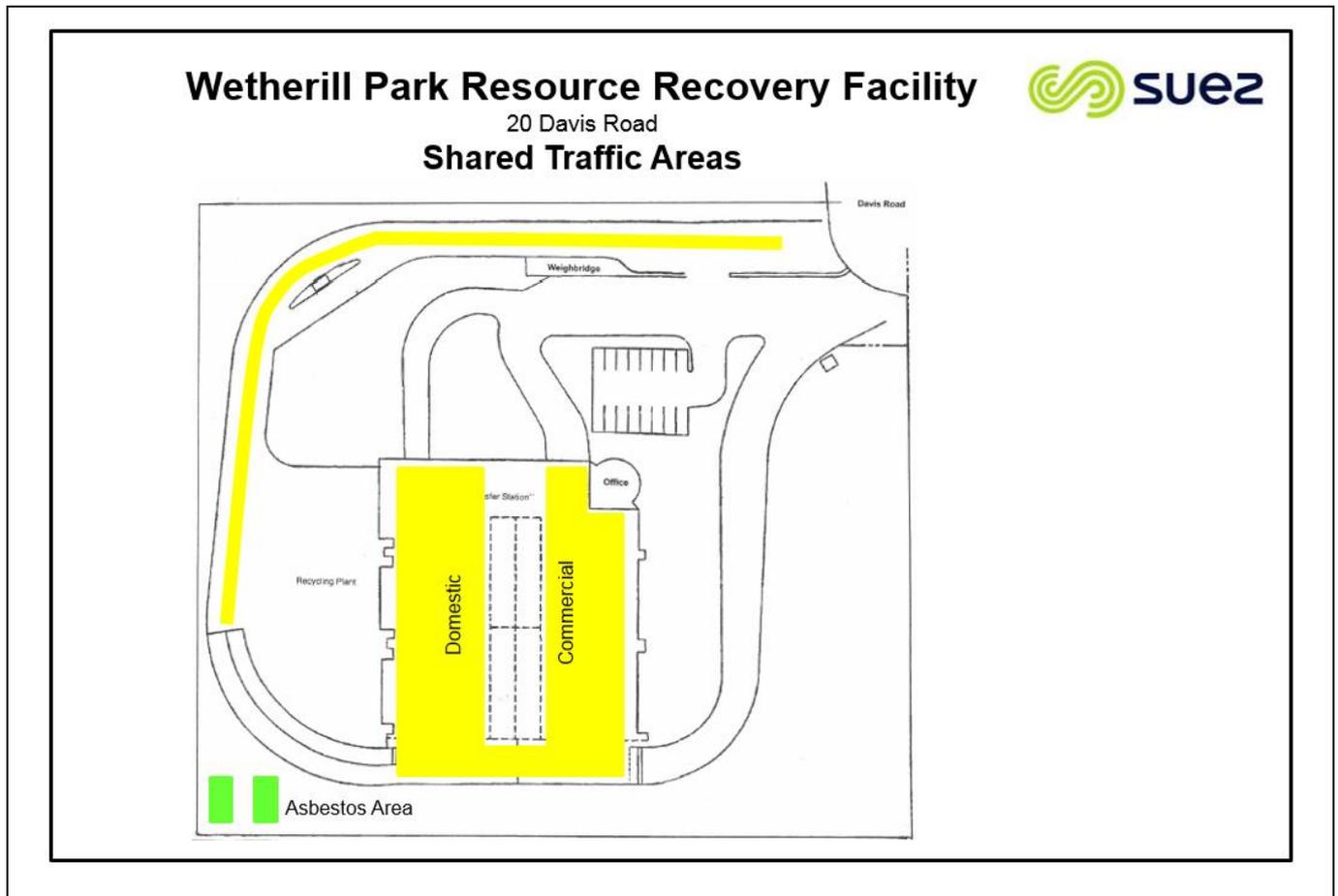


Diagram 5 – Shared Traffic Areas

2.8 Plant Traffic

- Plant traffic movement can be conducted on either side of the surge pit, except for the dozer which works within the surge pit
- For the refuelling of plant, they are to be parked up at the northern end of the surge pit near the fuel bower, out of the way of all vehicles entering the station
- Plant has access to the load out tunnel for cleaning purposes.
- All plant has the right of way
- The bobcat accesses all roadways for cleaning

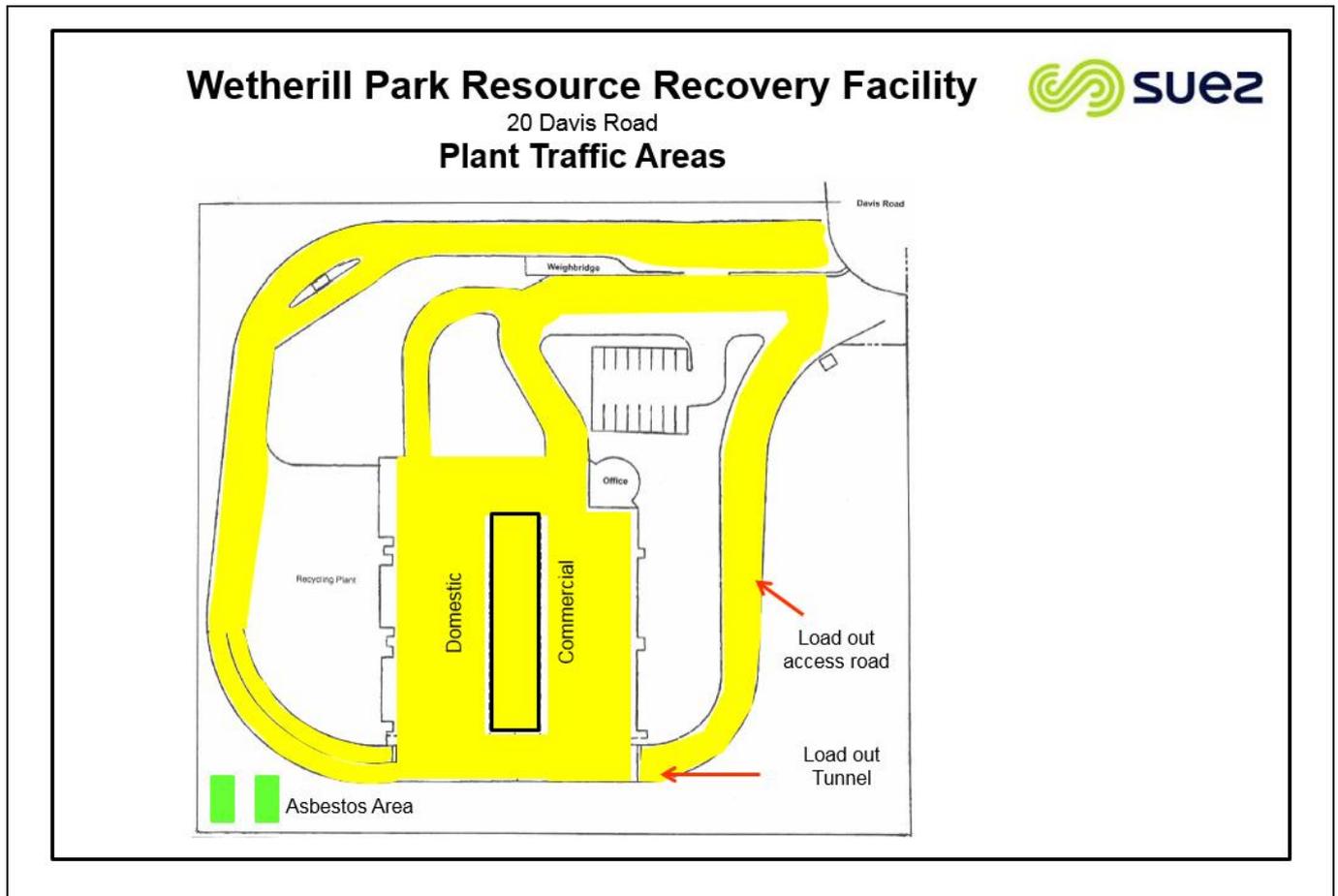


Diagram 6 – Plant Traffic Areas

2.9 Uncovering

- Uncovering of vehicles is conducted in the immediate delivery area for the particular waste being delivered. If recovering is required, this is completed in the same area prior to moving the vehicle.

2.10 Pedestrians

- Pedestrians on the sites are required to wear hi-visibility clothing at all times, with the exception of general public customers who are unloading in the Receiving Hall
- General public are not permitted to walk around any other part of the facility
- Pedestrians are required to adhere to all traffic signs displayed, follow general road safety rules whilst moving around the facility and obey all instructions given by Operator

2.11 Parking

- All SUEZ staff and visitors generally driving domestic vehicles or small delivery vans enter the site and take the second left into the carpark. These vehicles exit the carpark, cross the incoming traffic lanes and turn right to exit the site. Caution must be taken when crossing traffic lanes and joining the other vehicles exiting the site.
- All visitors are to sign in at the Weighbridge or Reception.
- All vehicles are to reverse into the parking spaces provided.
- All visitors are to sign out before leaving the site

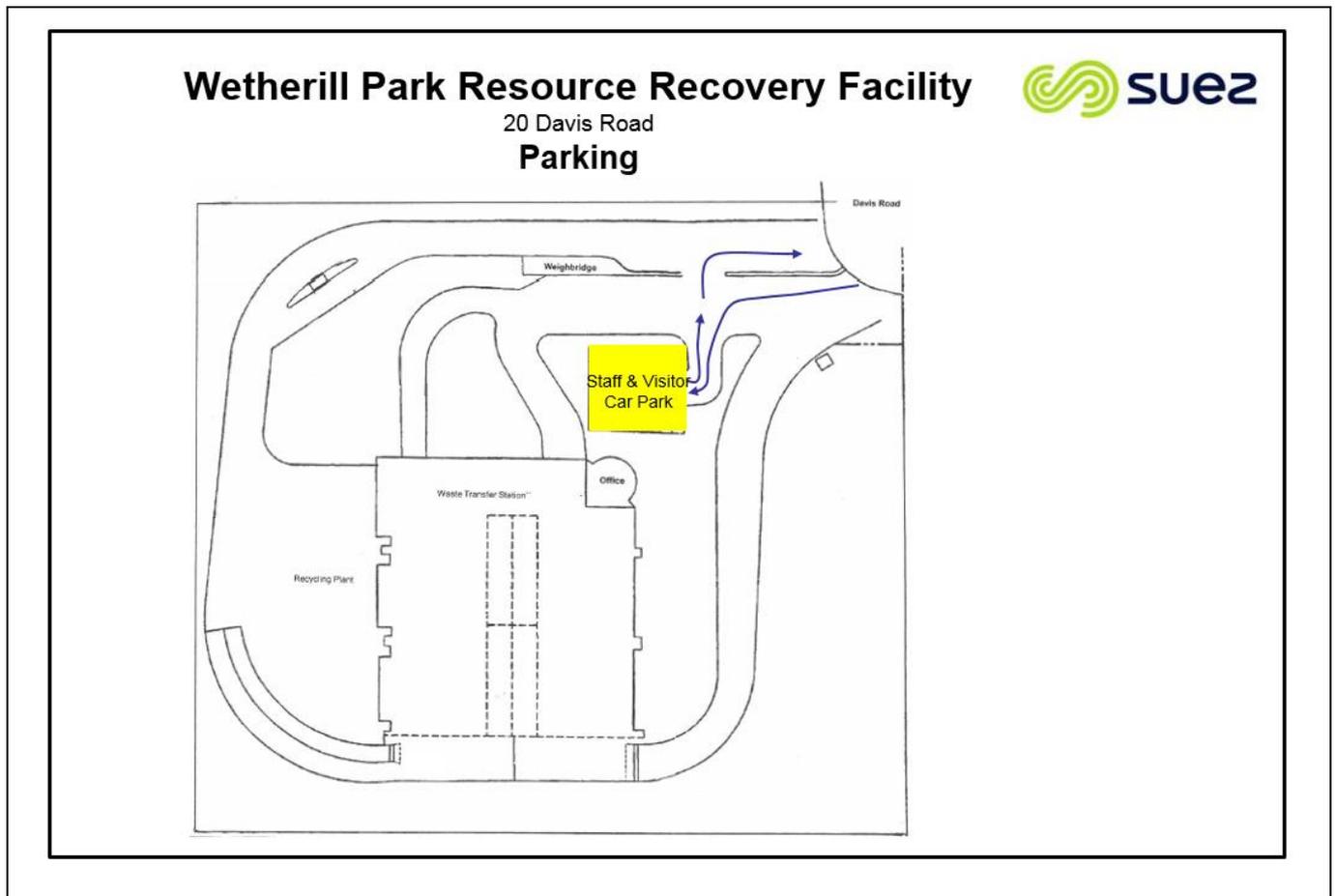


Diagram 7 – Parking

2.12 Storage

- There is a 2500 Litre dual skin diesel tank which is self bunded. This tank is located outside the western door (Gate 1) to the receival hall
- Hydraulic oil is stored inside the facility on bunded pallets and in a bunded area.
- Waste oil is collected and contained in a steal tank, which is self bunded

3. TRAFFIC ZONES – Normal Conditions

3.1 Light (Domestic) Vehicle Traffic

- Light (domestic) vehicles that contain general waste enter the site, and then cross the “In” weighbridge where they are directed to continue straight ahead and then enter the Receiving Hall through the western door (Gate 1). Customers are directed to the appropriate area for waste delivery by the Weighbridge Staff and Operators within the Receiving Hall (see diagram 2).
- After unloading their waste customers continue along the western side of the surge pit and join the traffic exiting the Receiving Hall through the southwest exit door, they will then merge with the transfer vehicles that are exiting the load out tunnel on the exit road. They then cross the “Out” weighbridge and exit the site.
- Asbestos loads are to be directed by the Weighbridge Staff to the Asbestos drop off area, they are to travel through the Receiving Hall and exit through the southwest corner, they then tip off at the asbestos area where they are supervised by an Operator.
- If a Domestic customer has a full load of clean cardboard they are instructed to enter the Receiving Hall via the eastern doorway (Gate 2) and to proceed to the south eastern corner to tip off their cardboard, the weighbridge operator will communicate this to the operators within the receiving hall via the 2-way

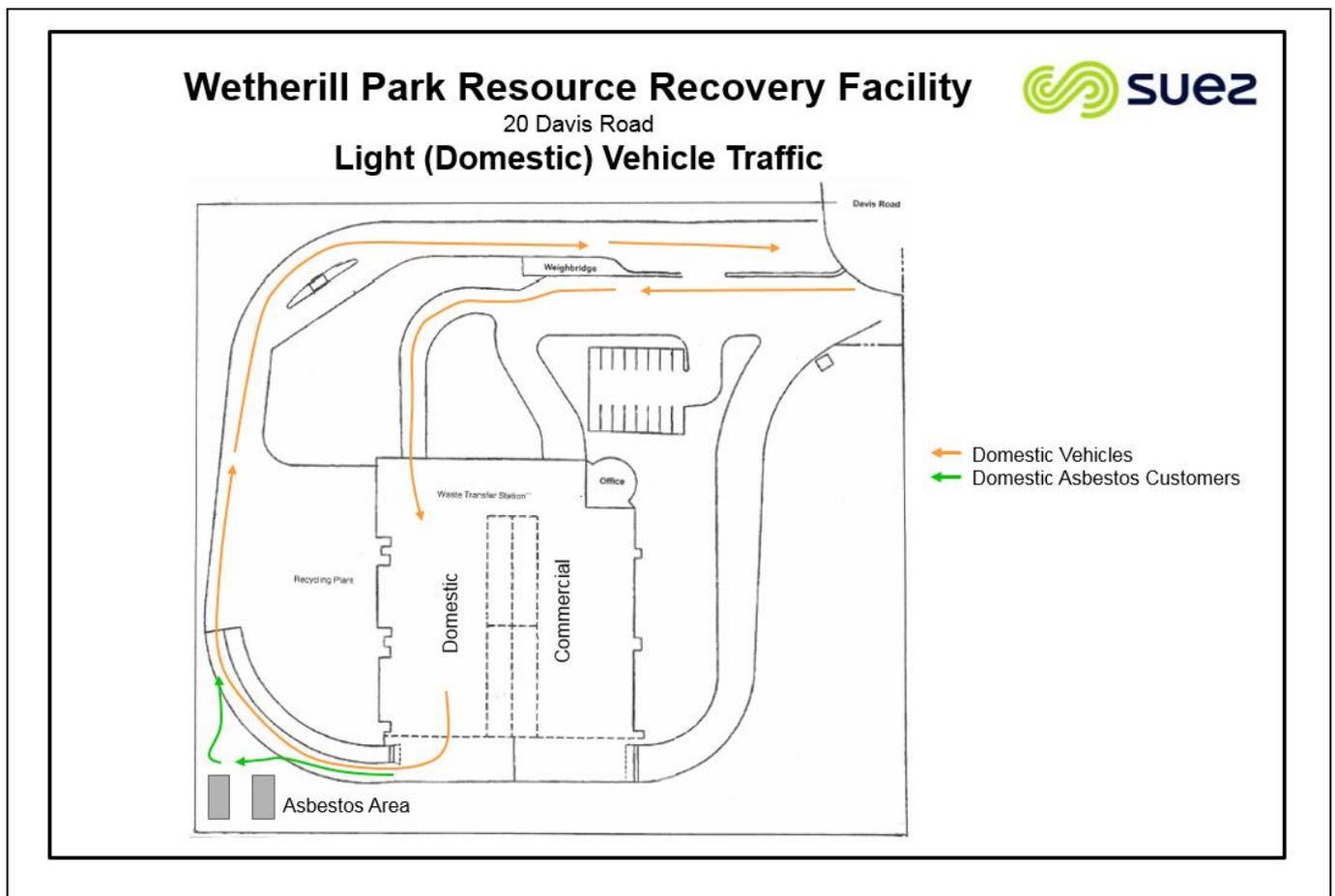


Diagram 8 – Light (Domestic) Vehicle Traffic

3.2 Commercial Vehicle Traffic

- Commercial vehicles enter the site and then cross the “In” weighbridge where they are directed to turn left and enter the Receiving Hall through the eastern door (Gate 2).
- After unloading they continue along the eastern side of the surge pit and pass behind the load out chute and join the domestic traffic exiting the Receiving Hall through the southwest exit door, they then merge with the Waste Transfer Vehicles that are exiting the load out tunnel on the exit road.
- They then cross the “Out” weighbridge and exit the site

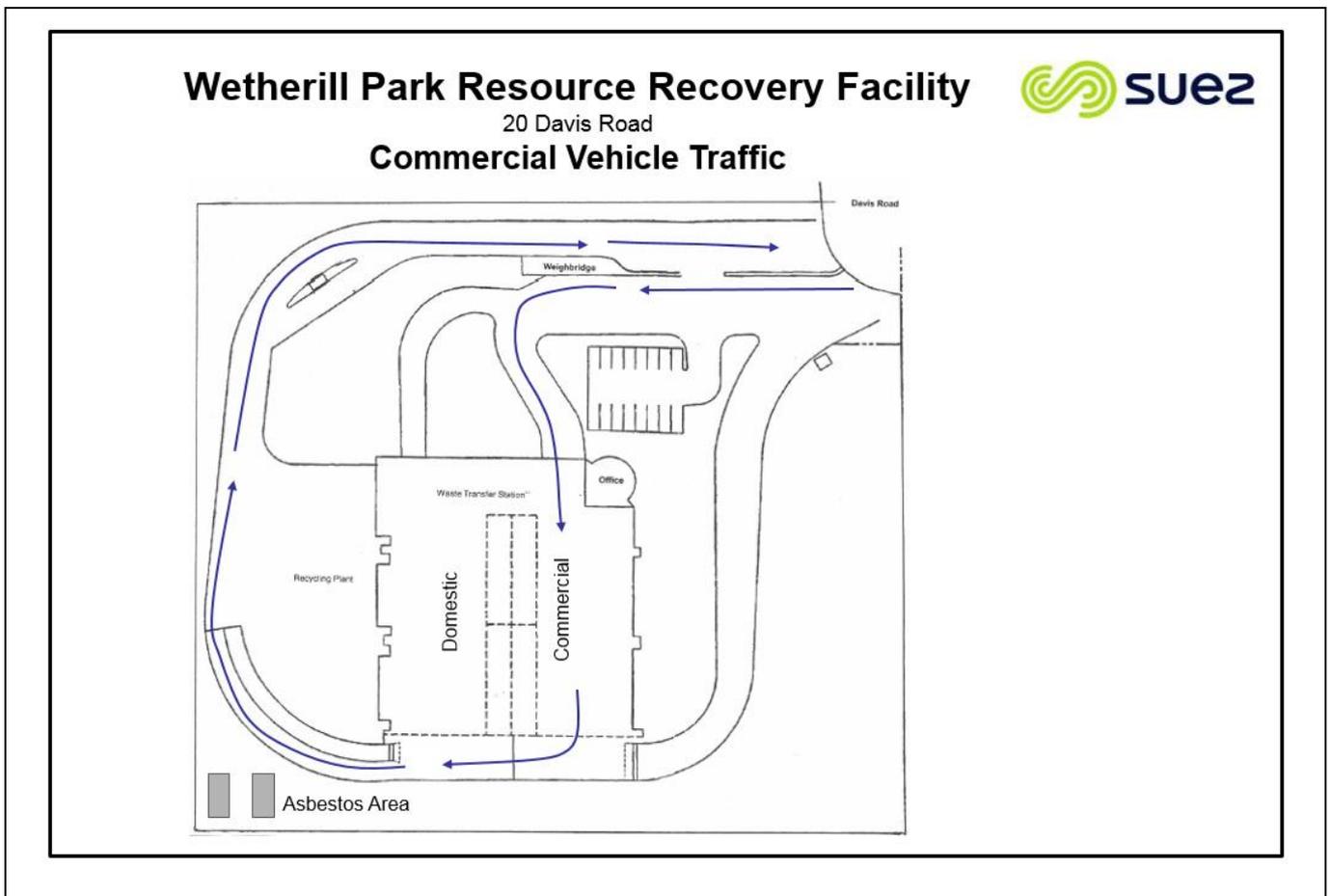


Diagram 9 – Commercial Vehicle Traffic

3.3 Waste Transfer Vehicle Traffic

- Waste Transfer Vehicles driven by SUEZ's Waste Haulage Contractor enter the site and immediately veer left to the load out tunnel access road.
- These vehicles travel down the access ramp (only after being given the all clear over the 2 way), they are then loaded in the tunnel and then exit out the tunnel via the exit ramp.
- They will then merge with the traffic stream exiting from within the Receiving Hall as they leave the ramp, they will clean off at the gantry then cross the "Out" weighbridge and exit the site

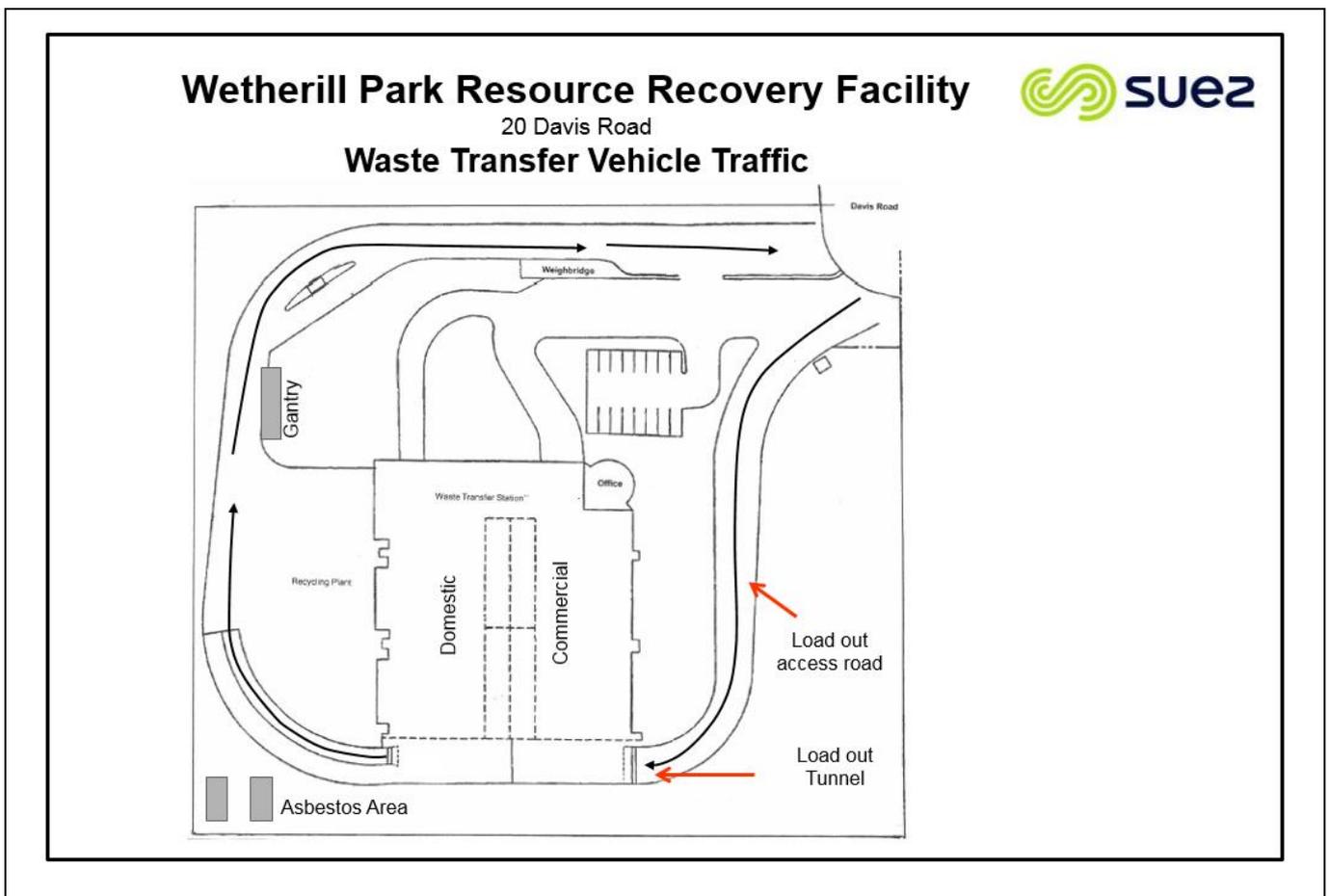


Diagram 10 – Waste Transfer Vehicle Traffic

4. TRAFFIC ZONES – Variations to Normal Conditions

- Variations to normal traffic movement have been identified, associated with over/under weight Waste Transfer Vehicles, diesel fuel deliveries or unserviceable weighbridges
- In the event that the “In” and “Out” weighbridges are both are unserviceable Customer Service is to be contact (1300 651 116)

4.1 Overloaded/Under Weight Transfer Vehicles

- Waste Transfer Vehicles that are observed to over/under weight exit the site, turn around in the Davis Road Cul-de-sac. They then re-enter the site and veer left onto the load out tunnel access road and travel down the ramp (only after being given the all clear over the 2-way). The excess waste is removed or additional waste is loaded. They then exit the tunnel via the tunnel exit ramp.
- They will then merge with the traffic stream exiting from within the Receiving Hall as they leave the ramp, they will clean off at the gantry again then cross the “Out” weighbridge and exit the site

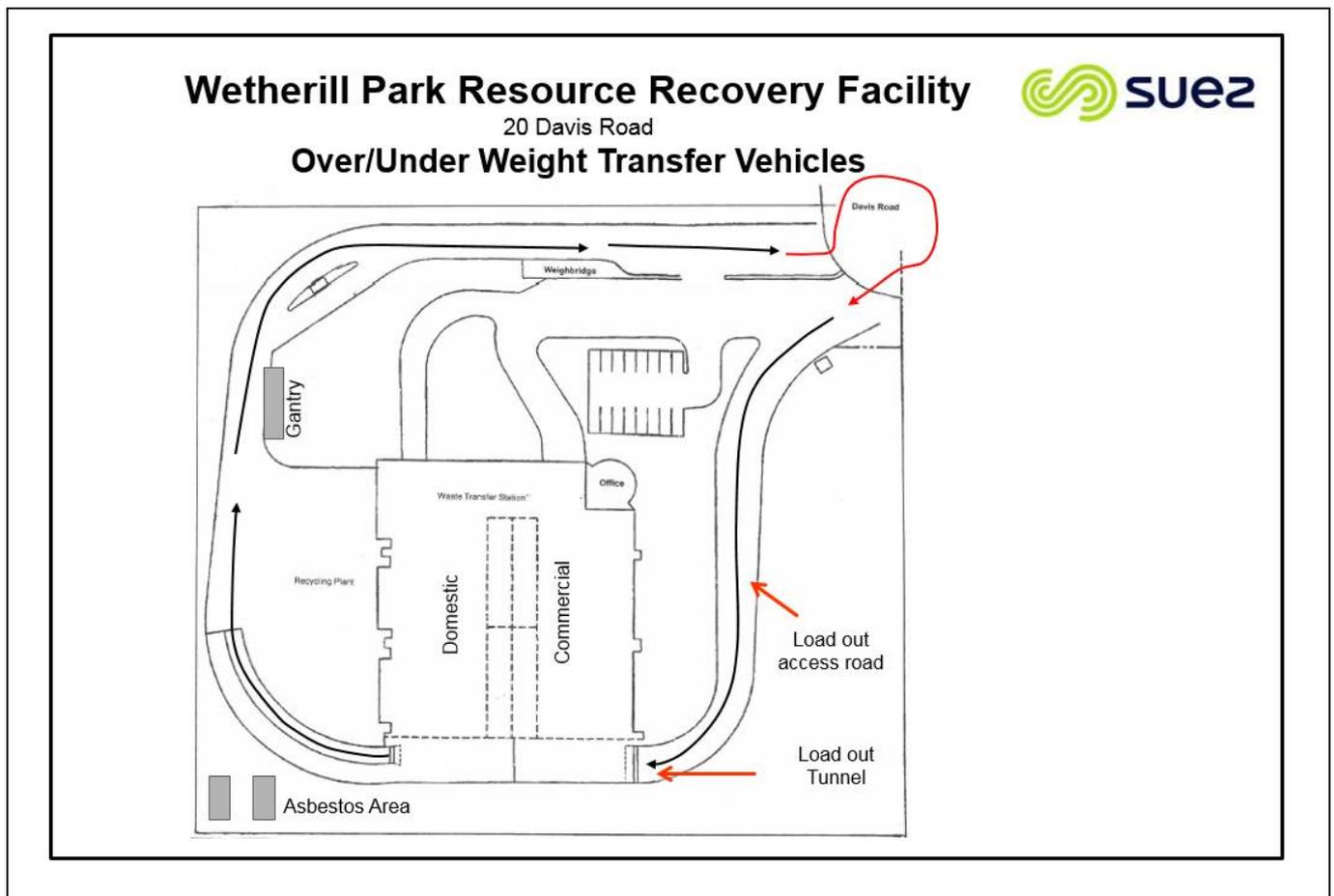
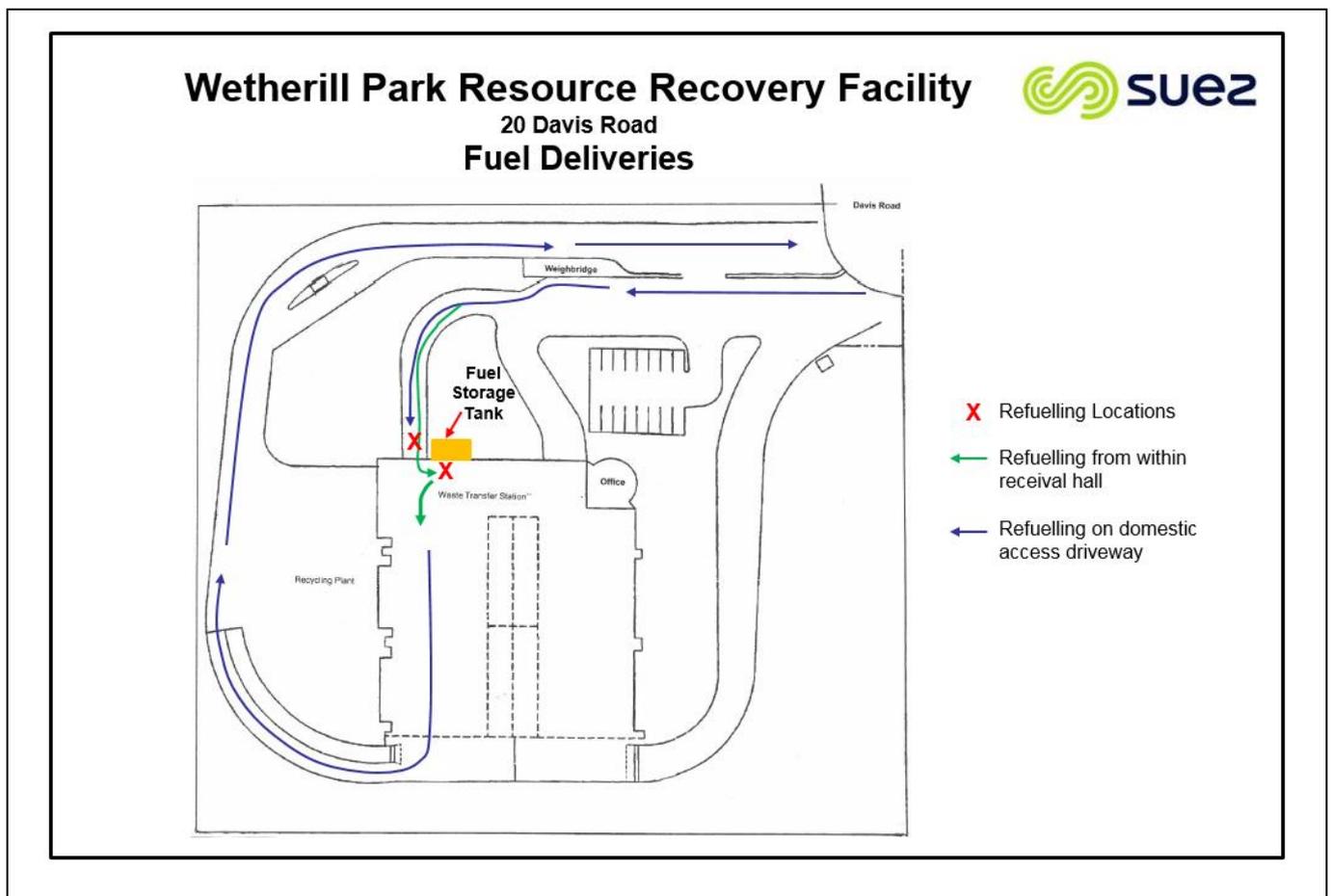


Diagram 11 – Over/Under Weight Waste Transfer Vehicles

4.2 Diesel Fuel Deliveries

- The fuel tanker will enter the site via the “In” weighbridge where they will sign in and are entered into Mandalay as a “Service Vehicle”.
- The fuel tanker will enter the Receiving Hall via the western door (Gate 1) and immediately turn to the left and park up. They will then refuel the tank.
- If the tanker is unable to park up just inside Gate 1, they are to park up on the domestic access road into the Receiving Hall next to the fuel tank, traffic is temporarily stopped until the fuel tanker has completed refuelling the tank.
- Upon completion of refuelling the tank they are to follow the flow of traffic and exit the Receiving Hall via the exit door in the south western corner. They will then proceed to the “Out” Weighbridge.
- The fuel tanker driver is to sign out, their Mandalay ticket is to be closed out and they are to leave the refuelling docket with the Weighbridge Operator and proceed to exit the site.



4.3 “Out” Weighbridge Unserviceable

- All Commercial and Domestic vehicles enter the site. Stop before the weighbridges and await the all clear from the traffic controller (No. 1) located at the entry of the weighbridges, they then proceed to the “In” Weighbridge. All vehicles are to turn the left and enter the Receiving Hall through the eastern door (Gate 2). They pass through the Receiving Hall on the eastern side of the surge pit, pass behind the load out chute and turn right up the western side of the surge pit and exit via the western doorway (Gate 1).
- Domestic vehicles are to tip their waste on the western side of the surge pit, and the Commercial vehicles are to tip their waste on the eastern side of the surge pit.
- When exiting all vehicles are to stop prior to the “In” Weighbridge at traffic controller (No. 2) located at the western end of the weighbridge, upon the all clear they are then to proceed to the “In” Weighbridge to be weighed, receive their ticket and then exit the site.
- Waste Transfer Vehicles are to exit the load out tunnel and exit the site via the “Out” weighbridge or “Out” Weighbridge bypass road (Under normal operations this bypass road is padlocked closed) into Davis Road. Under the control of traffic controllers (No. 1 and No. 2) they are to reverse up onto the “In” weighbridge to be weighed and receive their ticket, they are then to drive off and exit the site via the entry gate, still under the control of the traffic controller (No. 1)
- Traffic controllers are equipped with 2-ways and stop/go signs, they are located at the Davis Road entry (No. 1) and at the western end of the “In” Weighbridge (no. 2) to avoid conflict with the vehicles entering and exiting the site.
- Traffic Controllers are to stay in 2-way communication with the Weighbridge Operators and other Operators within the Receiving Hall.

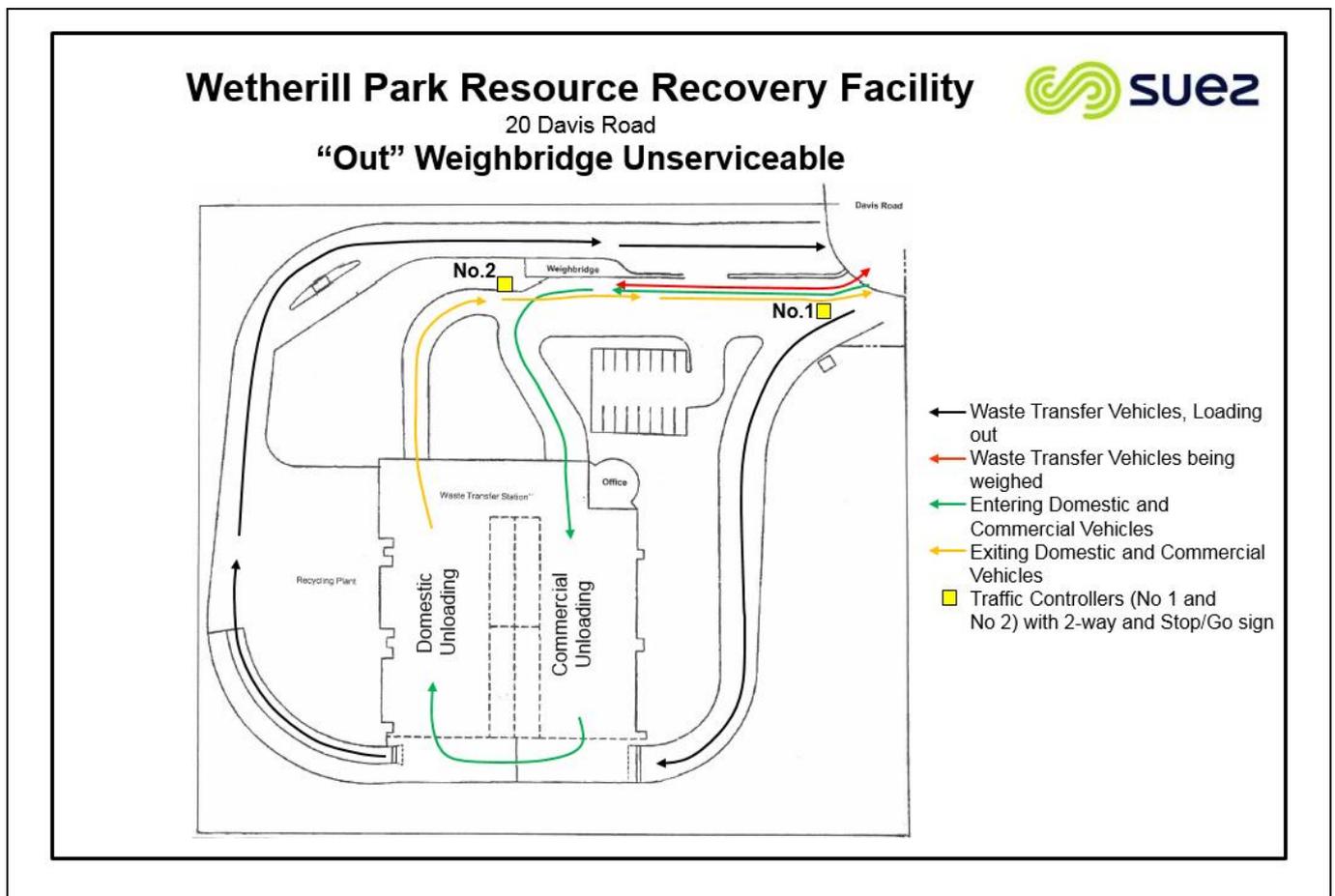


Diagram 13 – “Out” Weighbridge Unserviceable

4.4 "In" Weighbridge Unserviceable

- All Commercial and Domestic vehicles are to enter the site through the exit gate, stop at the traffic controller (No. 1) and await the all clear to proceed to the "Out" Weighbridge, they then follow the road to enter the Receiving Hall via the south-western exit doorway, where they await the all clear from the traffic controller (No. 3) prior to entering the Receiving Hall.
- Domestic vehicles turn left immediately upon entering the receiving hall to access the western side of the surge pit tipping area
- Commercial vehicles upon entering the receiving hall pass behind the load out chute and turn immediately left to the eastern side of the surge pit tipping area.
- After unloading both Commercial and Domestic vehicles await the all clear from the traffic controller (No. 3) and exit through the southwest next door, merging with the transfer vehicles exiting from the load out tunnel. They then proceed past traffic controller (No. 2) and stop at the "Out" weighbridge for payment and then exit the site.
- There is no change to normal traffic conditions for the Waste Transfer Vehicles, they must follow instructions given over the 2-way.
- Traffic Controllers are equipped with 2-ways and stop/go signs, they are located at the Davis Road entry/exit (No. 1), the north western corner (No. 2) and the southwest entrance/exit doorway (No. 3) to avoid conflict with vehicles entering, moving around and exiting the site.
- Traffic Controllers are to stay in 2-way communication with the Weighbridge Operators and other Operators within the Receiving Hall.

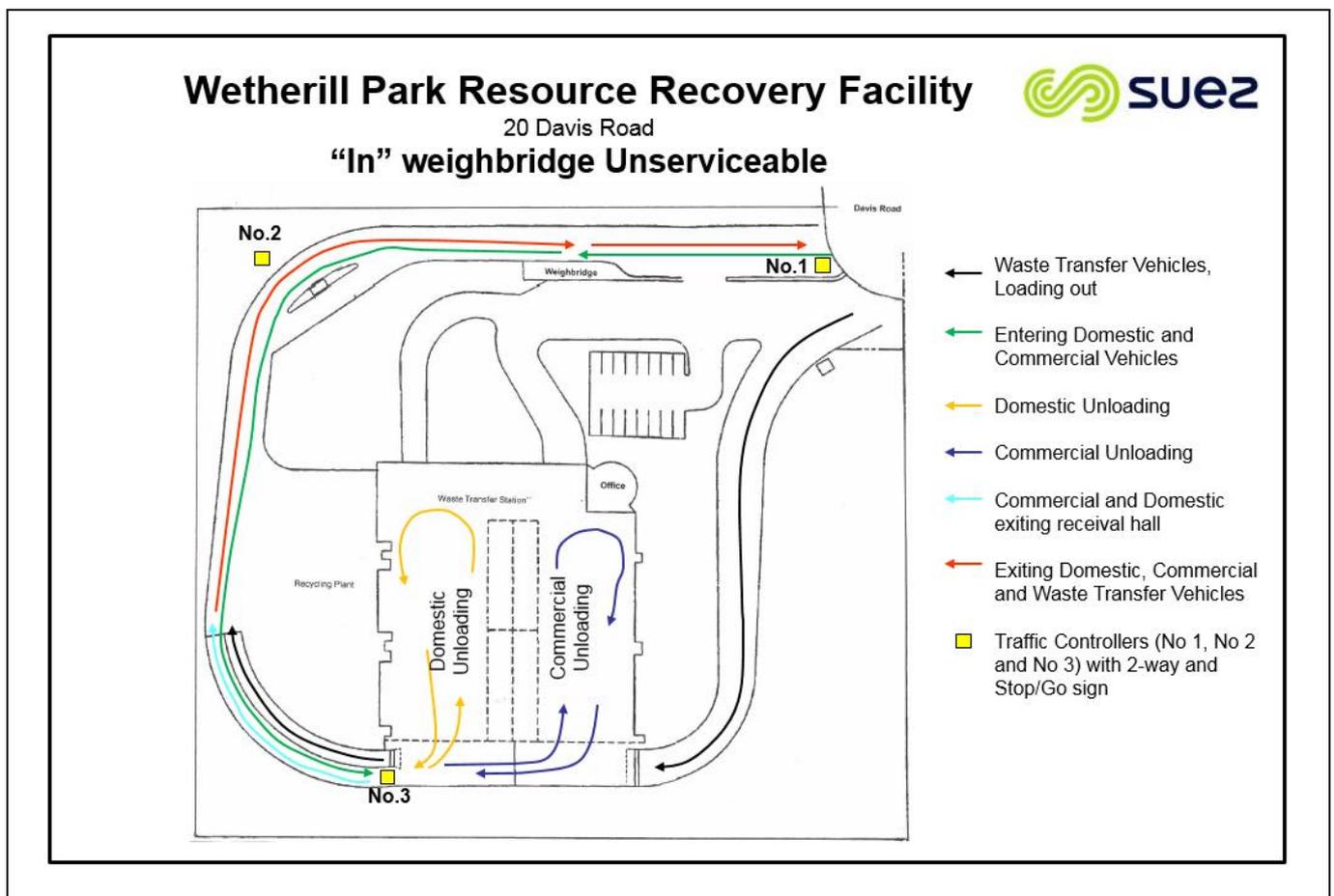


Diagram 14 – "In" Weighbridge Unserviceable

4.5 Both Weighbridges Unserviceable

- In the event that both weighbridges are unserviceable the site is to be closed until one or both of the weighbridges are serviceable. Customer Service is to be notified when the site is closed and then again when re-opens.
- Customer Service 1300 651 116

5. EVACUATION ROUTE

- In the event that an evacuation is required to evacuation alarm will sound
- The front entry gate is to be closed
- All Operators are to assist customers with evacuating the site
- Ensure that any children in vehicles are removed and evacuated with the adult that they came in with.
- All vehicles are to be checked that no one is left behind
- Customer in the Receiving Hall are to exit via the fire exits and then proceed to the Emergency Assembly Point (See *Diagram 15B*)
- Receiving Hall fire exit locations (See *Diagram 15A*)
 - 1. Northern Domestic entry doorway (Gate 1)
 - 2. Northern Commercial entry doorway (Gate 2)
 - 3. North eastern wall
 - 4. South eastern wall
 - 5. South western corner if unable to exit via South eastern wall
- Any Waste Transfer Driver in the load out tunnel is to exit via the eastern entry, unless blocked and then they are to exit via the western exit and follow the exit access road around to the Emergency Assembly Area (see *Diagram 15B*).
- The sign in register at the Weighbridge and Reception are to be taken to the Emergency Assembly Area to mark off all visitors.
- The Weighbridge Operator is to check Mandalay for the amount of vehicles remaining in the Receiving Hall
- The Weighbridge Operator is to exit out of the weighbridge and proceed along the “In” weighbridge to the Emergency Assembly Point (see *Diagram 15B*)
- All staff/visitors in the Administration building are to leave via the front reception door and to lock the back door preventing entry into the building. If the front reception door is blocked, then the rear door is to be used and to exit via the fire exit on the north eastern wall (see *Diagram 15B*).
- Any Operators/Contractors in the Recycling Plant/Workshop area are to follow the exit access road around to the Emergency Assembly Area (see *Diagram 15B*)
- Customer Service is to be contacted on 1300 651 116

Wetherill Park Resource Recovery Facility
20 Davis Road
Evacuation Route Inside Receiving Hall

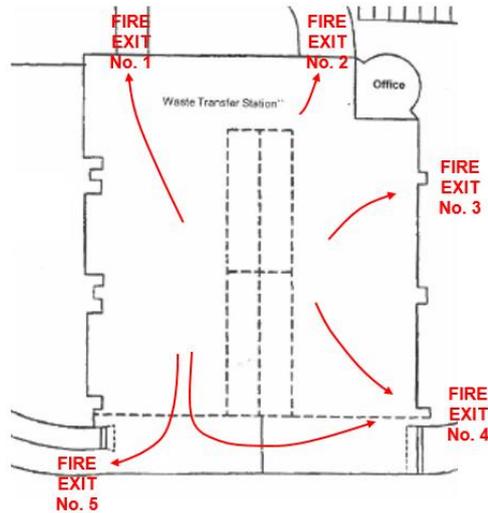


Diagram 15A – Evacuation from within Receiving Hall

Wetherill Park Resource Recovery Facility
20 Davis Road
Evacuation Route to Emergency Assembly Point

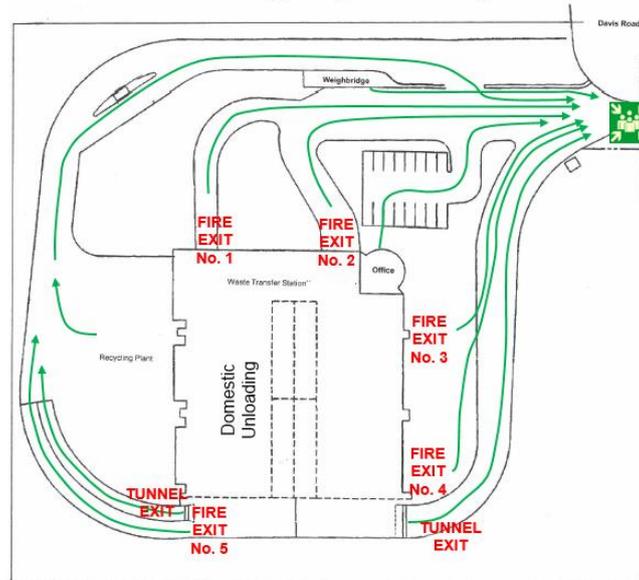


Diagram 15B – Evacuation route to Emergency Assembly Area

6. SAFETY INSTRUCTIONS

- The site normally has two weighbridges in operation (a southern/inbound and a northern/outbound) which are used together to support the separation of traffic within the station.
- The site speed limit is 10km/hour
- Under normal operations the site allows for one-way traffic
- All traffic is to stop at the “Stop” sign at the exit out onto Davis Road.
- Children must remain in vehicles at all times (except under evacuation procedures)
- Pets are not allowed outside of vehicles
- Enclosed shoes must be worn at all times
- Safety footwear and hi-visibility vest or clothing must be worn by all site staff, contractors and visitors being guided around the site
- A Permit to Work (FORM035) is required for all high risk tasks performed on site.
- All staff and visitors are to reverse park into the provided parking spaces
- Scavenging is not permitted
- Smoking is only allowed in the designated areas
- Use of mobile phone is not permitted on site
- Photos are not permitted without prior approval from the Site Manager.

7. TRAFFIC CONTROL SIGNAGE

Wetherill Park Resource Recovery Facility is open to both Commercial companies and to the general public for waste disposal. Traffic control signs are located at the entrance, throughout the site and at the exit to control all traffic. The following lists general and specific comments on the traffic control signage

- All drivers are to follow instructions from site staff.
- All drivers are to drive in a safe manner whilst on site and on the local approach road: Davis Road
- Where traffic control signage is covered by Australian Standards, these are used within the receival hall and around the site.
- At divide into the receival hall are “Gate 1” and “Gate 2” signs with directional arrows
- At the eastern (Gate 2) and western (Gate 1) entry doorways are the following signs:
 - “Tipping vehicles – unloading of waste”
 - “Stop”
 - “No smoking”
 - “Children to remain in vehicles”
 - “10” km/hour speed
 - “Surveillance”
- Entry to the “In” and “Out” weighbridges are controlled by a Stop Proceed with caution sign
- Speed on site is controlled within the receival hall and around the site by “10” km/hour speed limit signs
- A “no entry” sign is at the eastern end of the “Out” weighbridge
- A “stop sign controls traffic leaving from the truck wash area
- Exit from the eastern (Gate 2) and western (Gate 1) station entry doors are controlled by “Stop” and “No Exit” signs inside the station.
- Traffic flow within the receival hall is controlled by directional arrows and “Exit” signs located on the receival southern and western walls.
- Domestic vehicles leaving the asbestos area are controlled by a “Give way” sign
- Exit from the site is controlled by a “Stop” sign
- Potential conflict between Waste Transfer Vehicles exiting the load out exit ramp and Commercial/Domestic vehicles exiting from the receival hall is controlled by a “Give Way” sign facing the Commercial /Domestic vehicles.
- On the exit road from within the receival hall is a “One way” sign
- “Slippery when wet” sign in the south eastern corner of the receival hall assists in controlling the speed of traffic within the receival hall to the environmental conditions of the surface.
- When leaving the carpark there is a “No entry” sign to the entry gate and a “Give way” sign of traffic exiting the “Out” weighbridge
- Entry onto the tunnel load out access road is a “Restricted are – Do not enter” and “Loading tunnel under 24-hour video surveillance” signage.
- The use of speed humps throughout the site assist with controlling the speed of traffic on site
- Talking on mobile phones within the receival hall is not permitted
- In some emergency situations (e.g. fire/ flooding/ bomb threat) the entrance gate to the site is closed and the traffic on site is evacuated.
- In other circumstances (e.g. failure of “In” or “Out” weighbridge) traffic conflict on site is controlled by traffic controllers with 2-ways and stop/go signs

8. RESPONSIBILITIES

8.1 Site Manager

- Implementation of this plan
- Conforming to site rules.
- Training of staff in the plan.
- Communication of the plan.
- Reporting of incidents.
- Ensuring corrective actions are taken.

8.2 Site Supervisor

- Implementation of this plan
- Conforming with site rules
- Training of staff in the plan
- Communication of the plan
- Reporting of incidents
- Ensuring corrective actions are taken

8.3 Compliance Co-Ordinator (EQ&S)

- Ensuring adherence to this plan
- Conforming with site rules
- Training of staff in the plan
- Communication of the plan
- Reporting of incidents
- Implementing corrective actions

8.4 Weighbridge Staff

- Communicating to customers the correct unloading area
- Informing site manager/supervisor of non-conformity to the plan
- Reporting of incidents
- Can refuse entry to site after consulting manger/supervisor

8.5 Traffic Controller (when required)

- Ensuring adherence to this plan
- Conforming with site rules
- Reporting of incidents
- Implementing corrective actions

8.6 Site Staff

- Ensuring adherence to this plan
- Conforming with site rules
- Reporting of maintenance defects (worn signage)
- Reporting of incidents

8.7 Site Visitors and Contractors

- Ensuring their adherence to this plan, as advised at Induction
- Conforming with site rules
- Reporting of incidents

9. MAPS – SITE TRAFFIC FLOWS

Diagrams:

1. Traffic Flow
2. Waste Unloading Areas
3. Public Exclusion Zones
4. High Risk Traffic Volume Areas
5. Shared Traffic Areas
6. Plant Traffic Areas
7. Parking
8. Light (Domestic) Vehicle Traffic
9. Commercial Vehicle Traffic
10. Waste Transfer Vehicle Traffic
11. Over/Under Weight Transfer Vehicles
12. Fuel Deliveries
13. “Out” Weighbridge Unserviceable
14. “In” Weighbridge Unserviceable
15. A – Evacuation Route from within Receiving Hall
B – Evacuation Route to Emergency Assembly Area

Related Documents

DOCUMENT NAME	REFERENCE NUMBER
Emergency Response Plan	PLAN003

Review and Document Control

VERSION	CHANGE	REVIEWED	AUTHORISED	DATE ISSUED
2	Rebranded to SUEZ template. Updated to include Diagrams 3 to 7, and 15 A and B.	Site Manager	Int Sys Mgr	20/10/2016